

Section 2: Personal Travel by Mode

Introduction

This section outlines how patterns of travel and use of various modes of transport within Great Britain have changed over time. It also covers changes in the relative costs of different transport modes and in household expenditure on transport in the UK.

Car use has increased as disposable income has risen, against a backdrop of a small increase in the real cost of motoring and rising real costs of public transport fares. Although the average number of trips people make has declined over the last ten years, and the distance travelled is much the same, the time spent travelling has increased.

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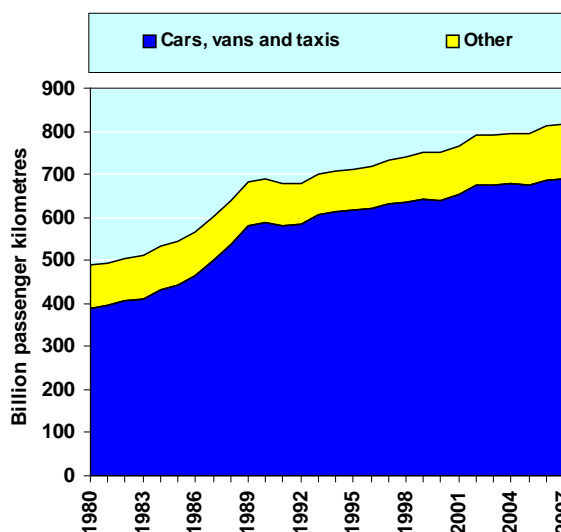
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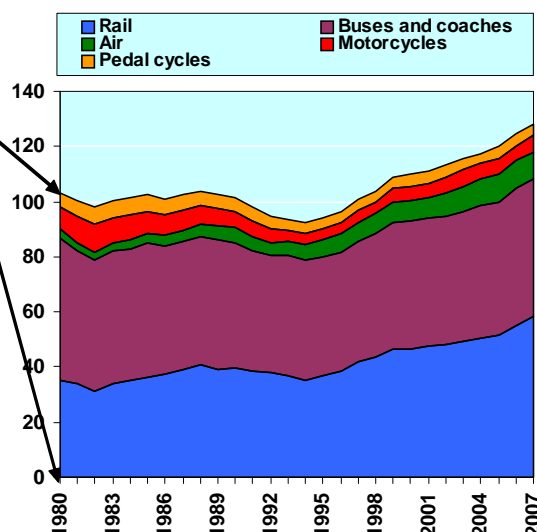
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2.1 Passenger travel by mode

Trend 2.1a – Passenger travel by mode: car and other modes: 1980 to 2007, Great Britain



Trend 2.1b – Passenger travel by modes other than car: 1980 to 2007, Great Britain



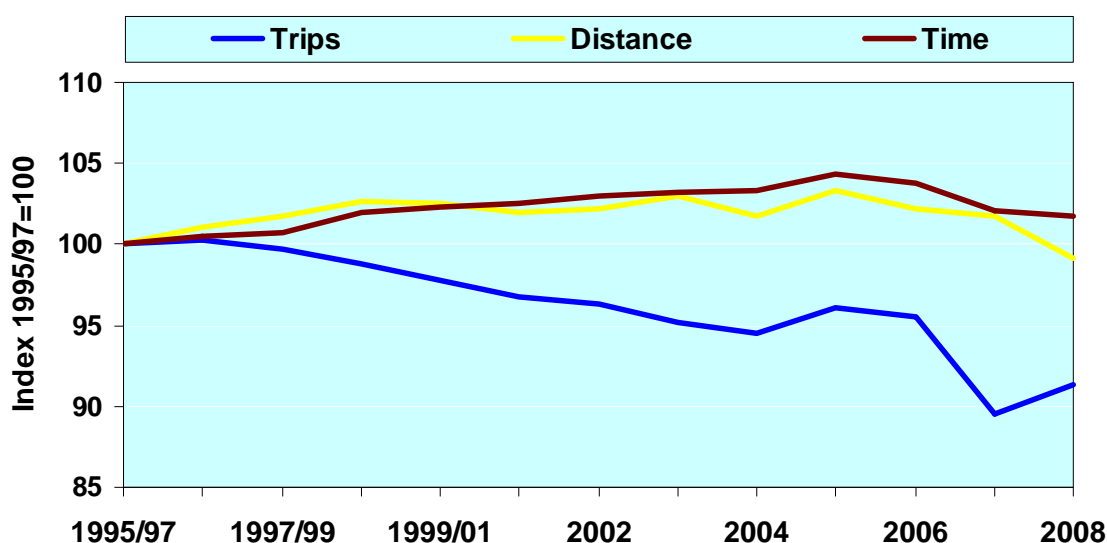
Source: Department for Transport, Office of Rail Regulation (rail) and Civil Aviation Authority (air)
 Rail and air data are outside scope of National Statistics

- Total distance travelled by domestic passengers increased by 66 per cent between 1980 and 2007, from 491 to 817 billion passenger kilometres. Much of this growth, an increase of 40 per cent, occurred between 1980 and 1990. Since then, distance travelled has increased by 18 per cent. Data for 2008 are not available for buses and coach passengers.
- The majority of the growth has been due to increased travel by car. The distance travelled by cars (including light vans and taxis) rose from 388 billion passenger kilometres in 1980 to 679 billion in 2008, an increase of 75 per cent.
- For travel modes other than car, the greatest percentage increase was in the distance travelled domestically by air, which trebled between 1980 and 2008. Travel by rail showed the largest absolute increase in distance, an additional 16 billion passenger kilometres, up by 67 per cent. Distance travelled by bus and coach fell by 18 per cent between 1980 and 1992. Although this has since increased by 17 per cent to 50 billion passenger kilometres in 2007, this still leaves it below the 1980 level.
- Between 1980 and 2008, the distance travelled by motorcycle fell by 30 per cent, although it has risen from a low point in the mid 1990s, whilst the distance travelled by pedal cycle has remained around 5 billion passenger kilometres.

The remaining charts in this chapter are produced from DfT's National Travel Survey (NTS). All results from the NTS in this publication are based on weighted data, and direct comparisons can only be made with *Transport Trends 2006* and later editions and not earlier editions. For more details see 'Notes and Definitions'.

2.2 Trends in travel

Trend 2.2 – Trends in travel: 1996 to 2008, Great Britain

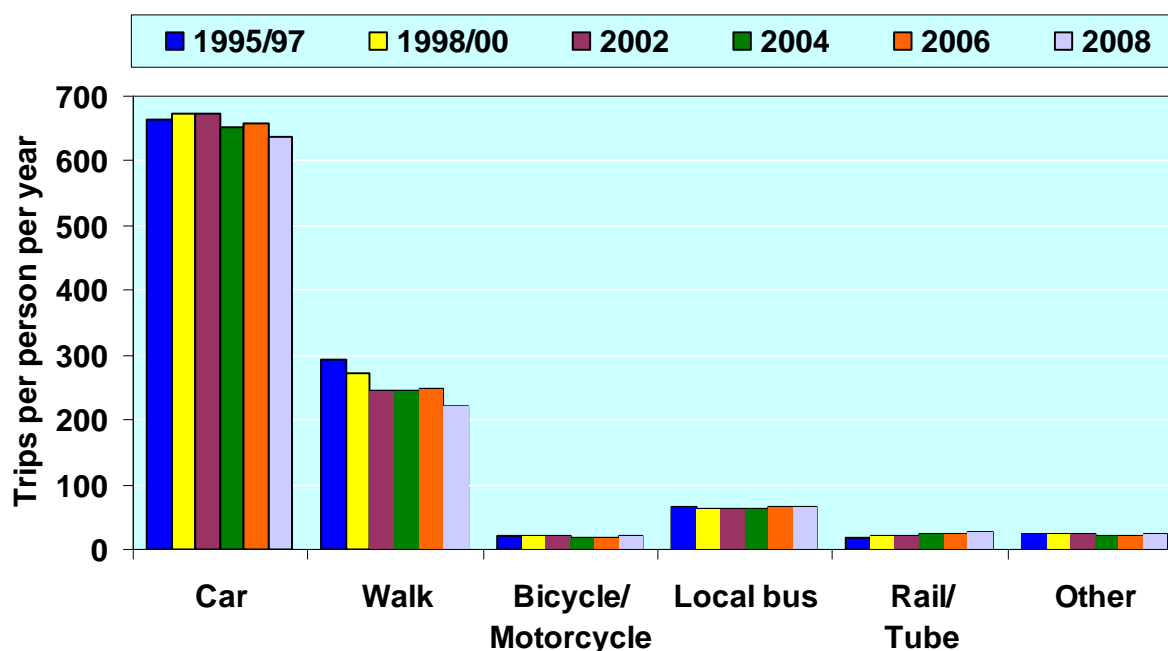


Source: National Travel Survey, Department for Transport

- On average, people are travelling about the same distance, spending slightly longer travelling but making fewer trips in 2008 than in 1995/97.
- Since 1995/97, the average distance travelled has fallen slightly by 1 per cent to 6,923 miles per person per year. Over the same period, the number of trips per person per year fell by 9 per cent to 992 trips per year, while the average time spent travelling by people increased 2 per cent to 376 hours per person per year, around an hour a day.

2.3 Passenger trips by mode

Trend 2.3 – Average annual trips made by mode: 1995/97 to 2008, Great Britain

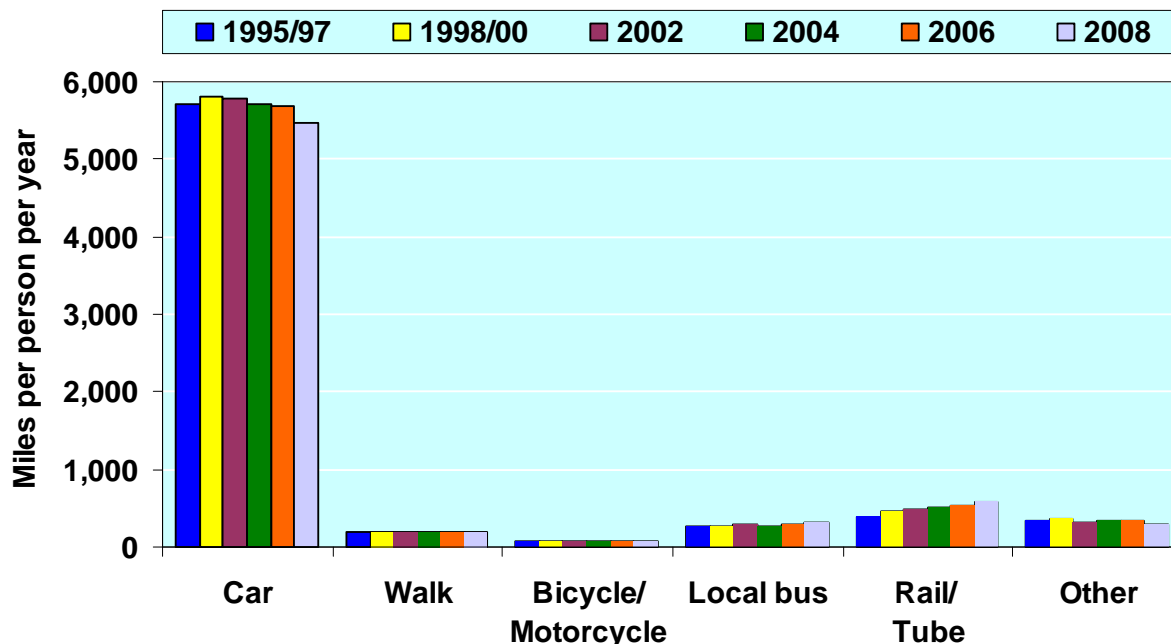


Source: National Travel Survey, Department for Transport

- The proportion of trips made by car increased slightly from 61 per cent in 1995/97 to 64 per cent in 2008. The average number of annual trips people made by car was 637 in 2008, compared to 664 in 1995/97.
- Over the same period, the number of trips made on foot declined by 24 per cent from 292 to 221 trips per person per year. Trips by bicycle or motorcycle varied only slightly across the years at around 20. Note that short walks are believed to be under-recorded in 2002 compared with other years and short trips under recorded in 2007 and 2008.
- Trips made by rail or London Underground increased 42 per cent from 19 to 27 per person per year between 1995/97 and 2008. However, the number of trips made by local bus remained at around the mid sixties.

2.4 Distance travelled by mode

Trend 2.4 – Average annual distance travelled by mode: 1995/97 to 2008, Great Britain

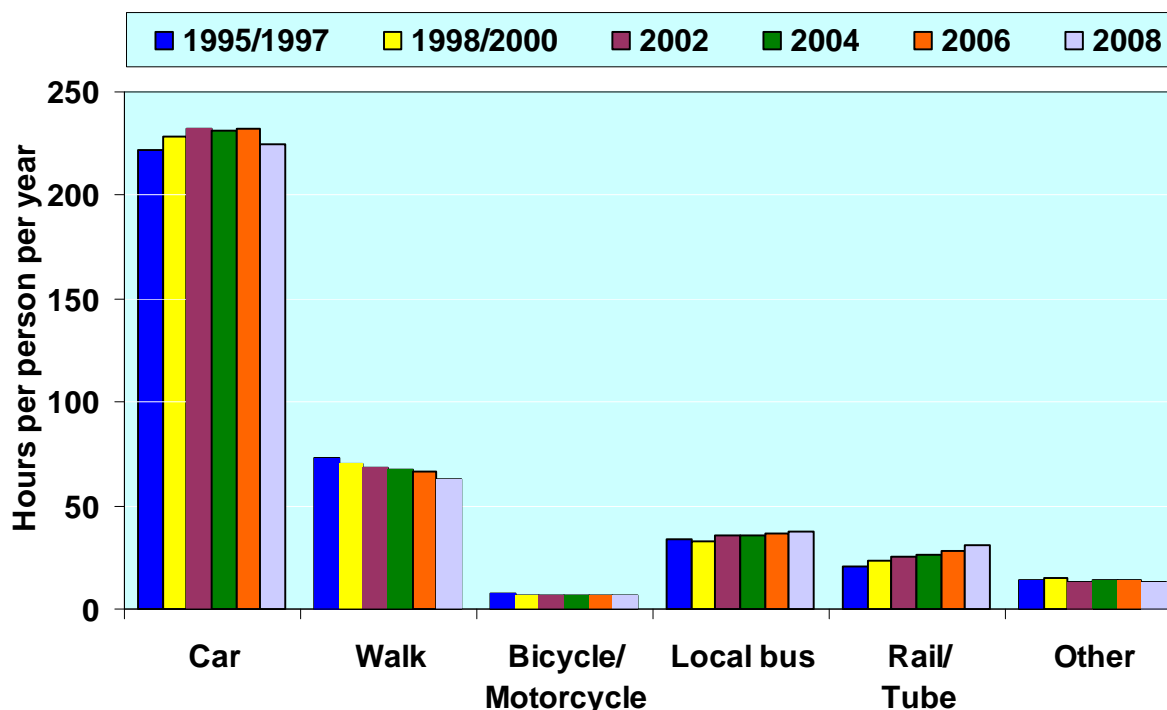


Source: National Travel Survey, Department for Transport

- The average annual distance travelled per person by car in 2008 was 5,468 miles, a fall of 4 per cent from the 1995/97 figure.
- Over the same period, the average annual distance walked also fell by 4 per cent to 193 miles per year, while the distance travelled by bicycle or motorcycle rose slightly from 78 to 80 miles per year.
- Distance travelled by local bus increased by 16 per cent, from 268 to 312 miles per person per year between 1995/97 and 2008. The distance travelled by rail/tube increased by 50 per cent from 380 to 570 miles a year

2.5 Time spent travelling by mode

Trend 2.5 – Average annual time spent travelling by mode: 1995/97 to 2008, Great Britain

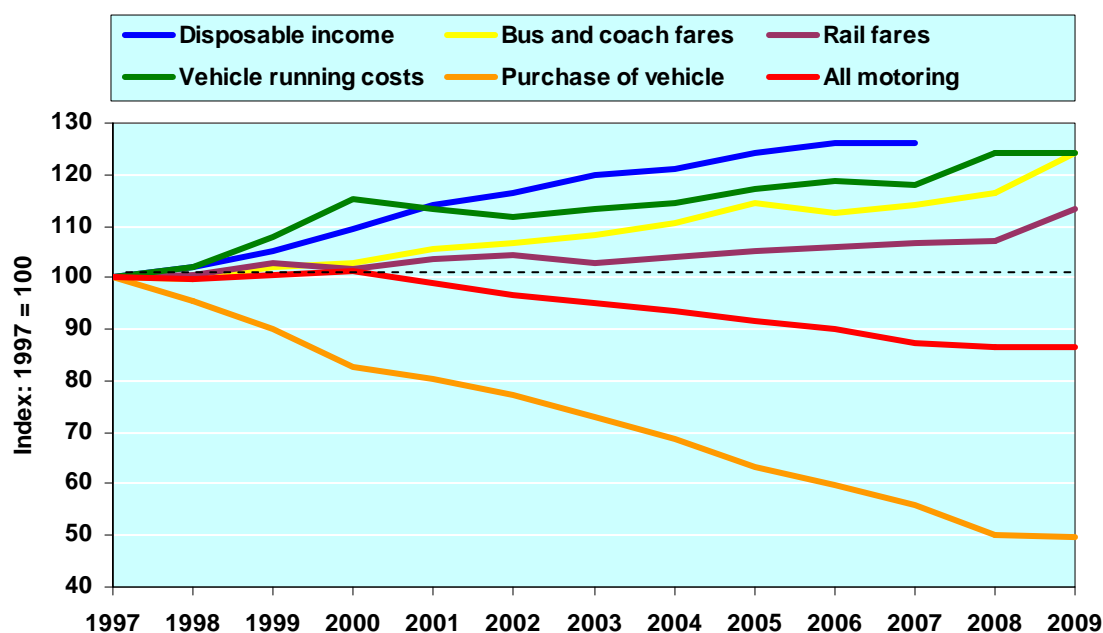


Source: Department for Transport

- Between 1995/97 and 2008, the average time people spent travelling increased by 2 per cent to 376 hours per person per year.
- Over this period, the average amount of time spent travelling by car increased marginally to 225 hours and the time spent walking decreased by 13 per cent to 63 hours. The amount of time spent travelling by rail and tube increased by 54 per cent to 31 hours.
- In 2008, travel by car accounted for 60 per cent of time spent travelling, the same as in 1995/97. Over the same period, the proportion of time spent walking has fallen from 20 to 17 per cent.

2.6 Changes in relative costs of transport

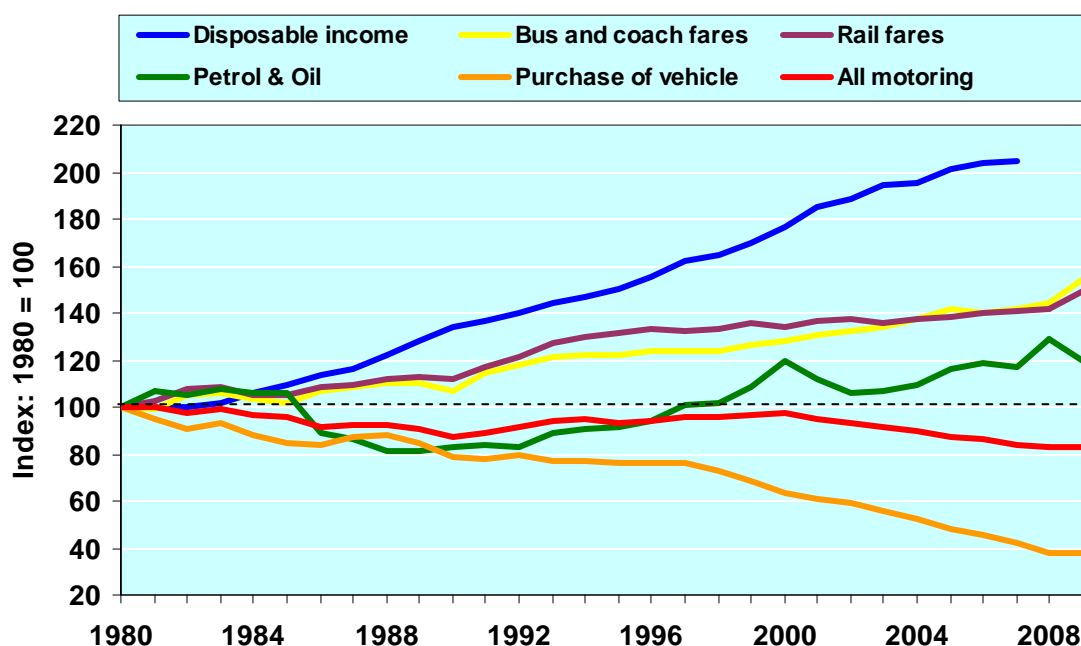
Trend 2.6a – Changes in the real cost of transport and in income: 1997 to 2009, United Kingdom



Source: Office for National Statistics

- The growth in car travel has been accompanied by a reduction in motoring costs and rising bus and rail fares in real terms.
- The overall cost of motoring (including purchase, maintenance, petrol and oil, and tax and insurance) has dropped below its 1997 level in real terms.
- The real cost of running a car (the cost of motoring excluding the purchase of a vehicle) increased by almost a quarter between 1997 and 2009 whereas the real cost of vehicle purchase has halved over the same period.
- Public transport fares have risen in real terms since 1997. In 2009, bus and coach fares and rail fares were respectively 24 per cent and 13 per cent higher than in 1997.
- Over the period to 2007, average disposable income increased by more than a quarter in real terms. Transport by any mode has therefore become more affordable, with a greater improvement in the affordability of car use than that of public transport.
- The vehicle running costs index, which excludes the cost of vehicle purchase, is only available reliably from 1997. For a longer trend see Trend 2.6b which includes the real cost of petrol and oil.

Trend 2.6b– Changes in the real cost of transport and in income: 1980 to 2009, United Kingdom

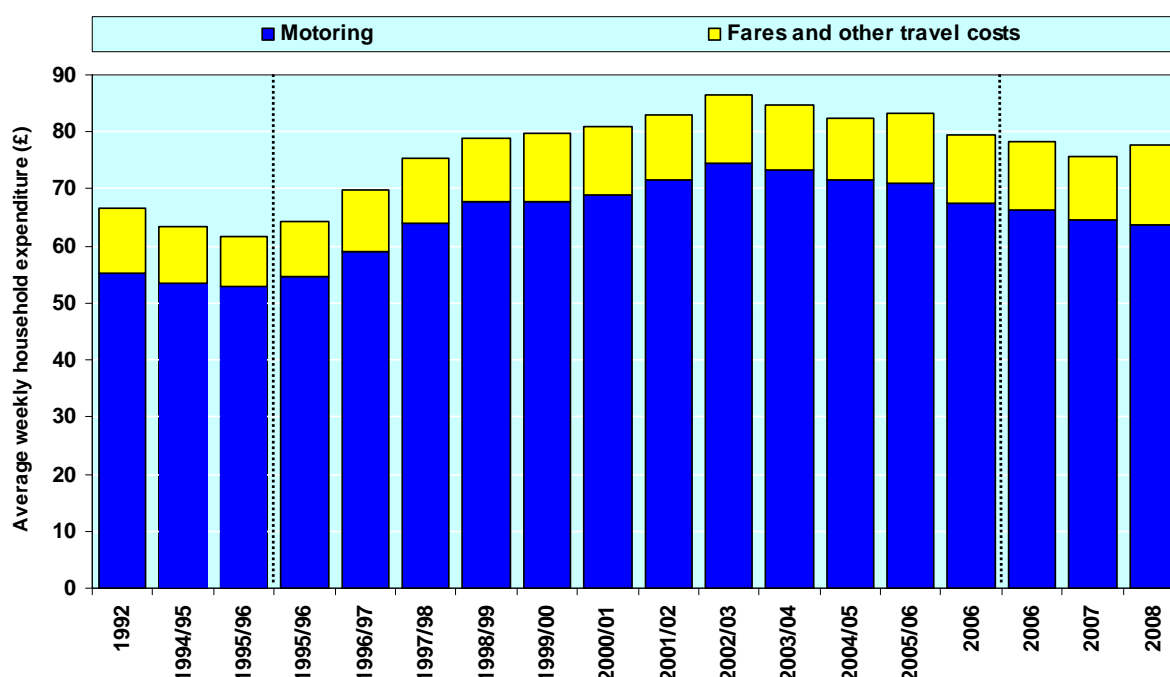


Source: Office for National Statistics

- A similar picture emerges over the longer term.
- The overall cost of motoring (including purchase, maintenance, petrol and oil, and tax and insurance) has dropped below its 1980 level in real terms with this fall partly driven by the fall in the real cost of vehicle purchase of over 60 per cent over the same period. The real cost of petrol and oil has increased by 20 per cent over this period.
- Public transport fares have risen in real terms since 1980. In 2009, bus and coach fares and rail fares were respectively 54 per cent and 50 per cent higher than in 1980.
- Over the period from 1980 to 2007, average disposable income more than doubled in real terms. Transport by any mode has therefore become more affordable, with a greater improvement in the affordability of car use than that of public transport.

2.7 Household expenditure

Trend 2.7 – Travel expenditure at 2008 prices: 1992 to 2008, United Kingdom

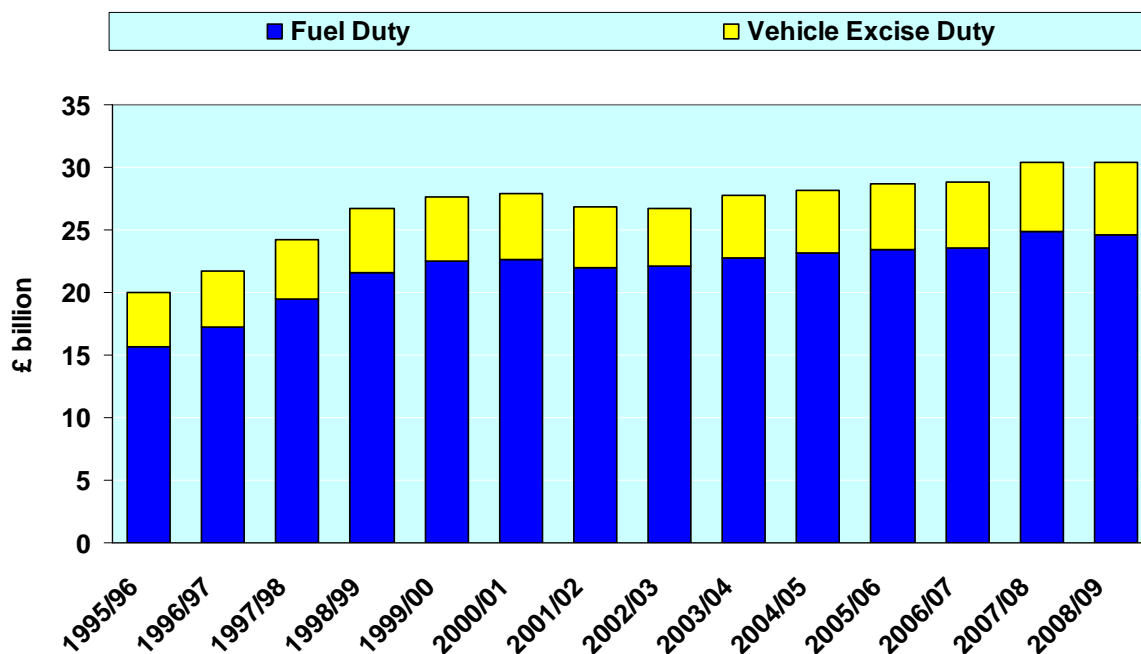


Source: Office for National Statistics
Family Spending: 2009 Edition

- Real spending on travel has increased since 1992, although costs have gone down since 2002/03 according to figures from the Living Costs and Food Survey (formerly the Expenditure and Food Survey). Between 1992 and 2008, spending on motoring at constant prices rose by 15 per cent whereas that on fares and other travel rose by 27 per cent.
- The increase in household expenditure on motoring reflects the increase in the number of cars owned per household. Household expenditure per car in real terms changed little over this period up to 2007. However there was a 15 per cent increase in the average weekly expenditure on petrol, diesel and other motor oils between 2007 and 2008.
- In 2008, households spent on average £78 per week on transport and travel (including motor vehicle insurance and taxation).
- Spending on motoring as a proportion of total household expenditure was 14 per cent in 2008 compared with 14 per cent in 1990 and 13 per cent in 1984.

2.8 Taxes and Charges on Road Users

Trend 2.8 – Fuel duty and vehicle excise duty receipts 1995/96 to 2008/09, United Kingdom



Source: HMRC and DVLA

The data in this table are outside the scope of National Statistics

- Vehicle Excise Duty receipts have increase by around third from £4.3 billion to £5.8 billion over the period from 1995/96 to 2008/09. Over the same period Fuel Duty receipts have increased by over a half from £15.7 billion to £24.6 billion.